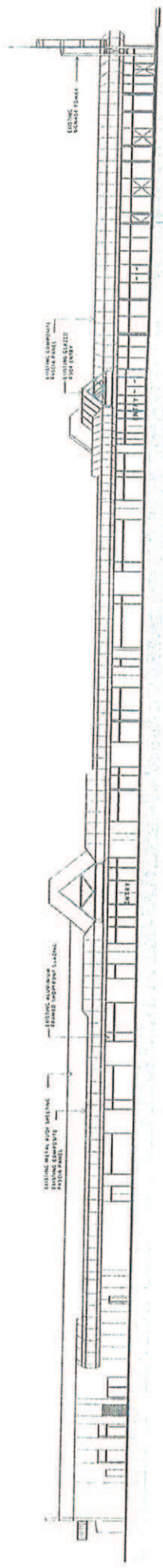
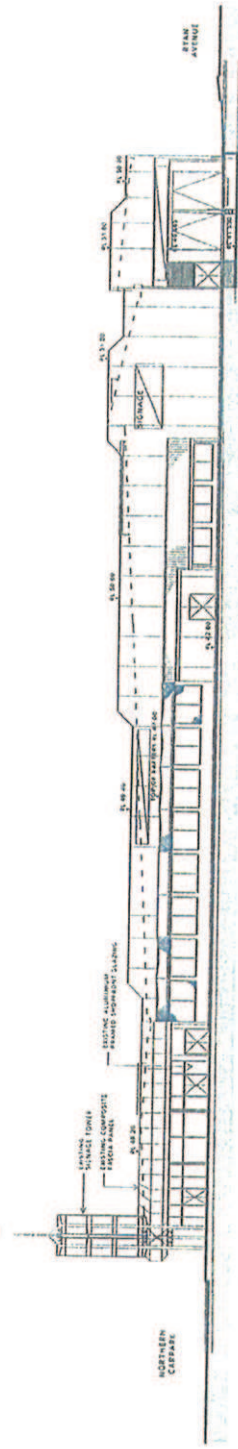


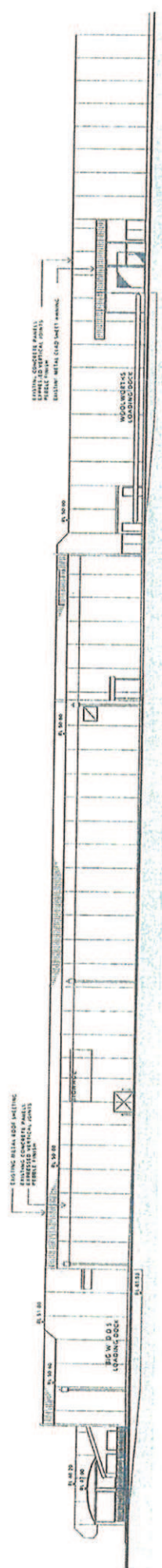
APPENDIX B – Plans and Elevations



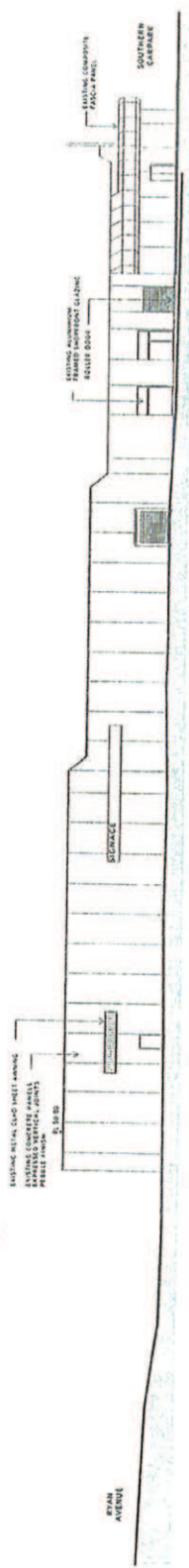
EXISTING SOUTH-EAST ELEVATION
SCALE 1:200



EXISTING NORTH-EAST ELEVATION
SCALE 1:200



EXISTING NORTH-WEST ELEVATION
SCALE 1:200



EXISTING SOUTH-WEST ELEVATION
SCALE 1:200

Existing Elevations
1:200

SCOTTCARVER



Gowrie Street Mall

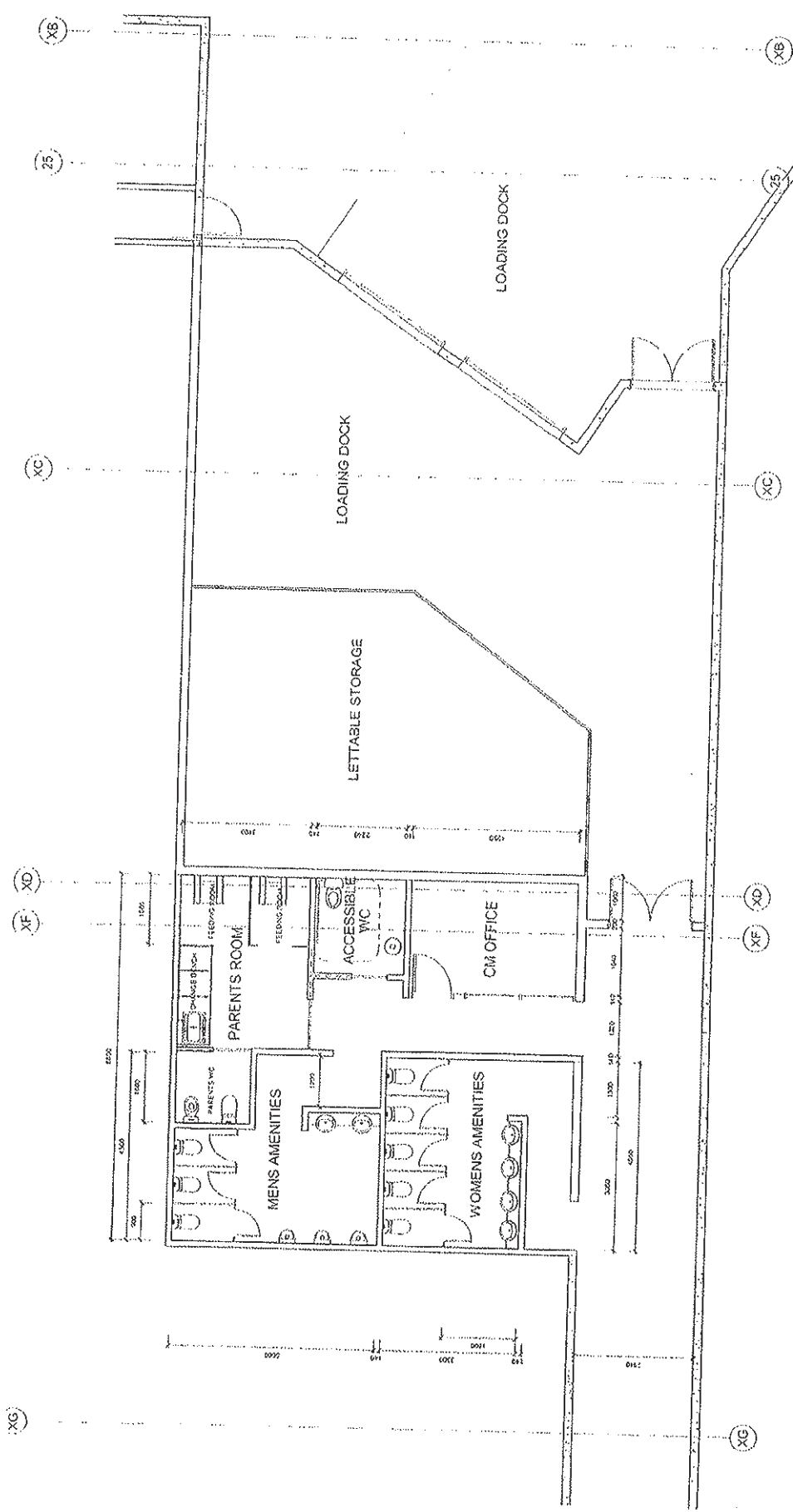


1:200 @ A1

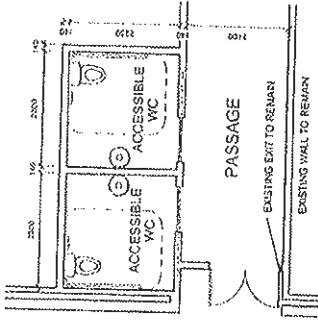
Existing Elevations

14/07/10

DA03 A

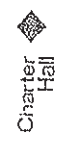


1 Detail - Proposed Amenities
1:50



2 Detail - Proposed Amenities 2
1:50

SCOTTCARVER



Gowrie Street Mall



1:50 @ A1

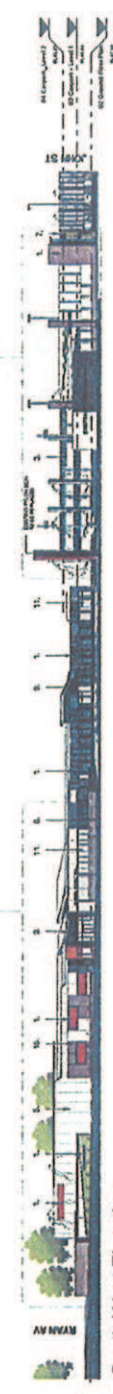
Proposed Amenities



1 John St Elevation
1:500



2 South-East Elevation
1:500



3 South-West Elevation
1:500



4 North-West Elevation
1:500

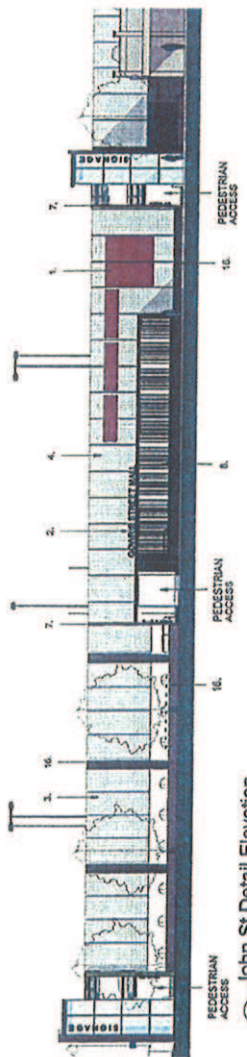


5 North-East Elevation
1:500

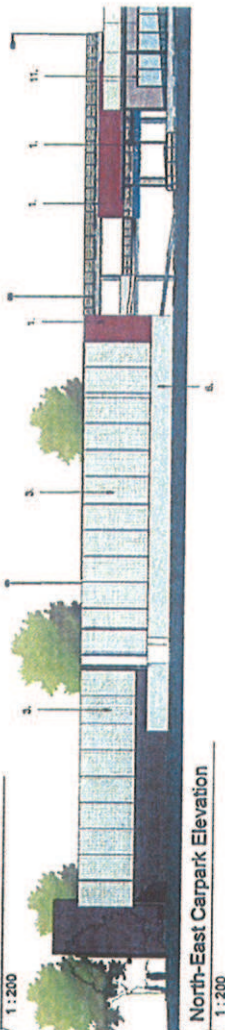
LEGEND

1. Signage Zone to future detail;
2. Existing Shopfront to future detail;
3. Galvanized steel frame and mesh panels;
4. Galvanized steel frame and perforated metal panels;
5. Existing concrete panels wall to remain;
6. New concrete panel wall;
7. Painted precast panels or blades;
8. Painted steel columns;
9. Metal Louver powdercoat finish;
10. Existing aluminium frame shopfront glazing to remain;
11. New aluminium framed shopfront glazing powdercoat finish;
12. Metal deck sheeting;
13. Glass balustrade;
14. Laminated timber soffit;
15. Exposed aggregate concrete paving;
16. Brick wall;

No.	Description	Date
A	DA Submission	13/07/2010
B	Amended DA	30/11/2010



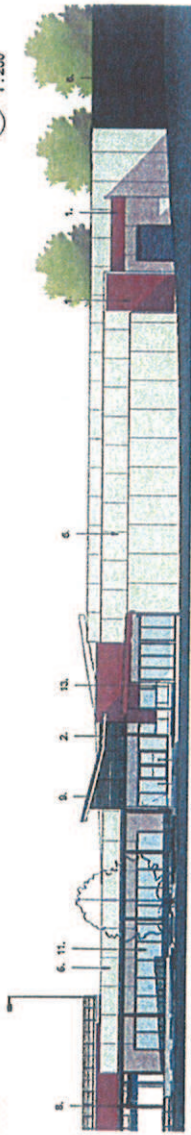
1 John St Detail Elevation
1:200



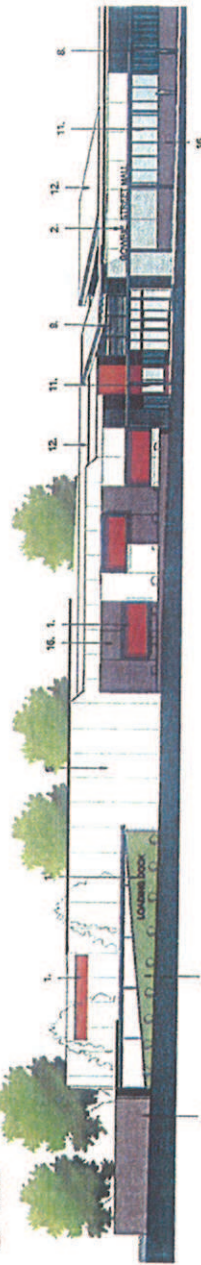
3 North-East Carpark Elevation
1:200



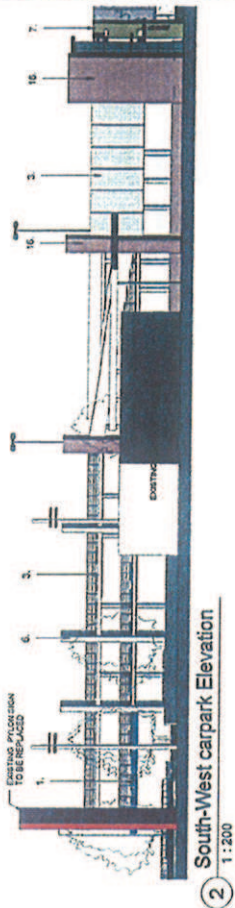
4 South-East Detail Elevation
1:200



6 North-East Detail Elevation
1:200



7 South-West Detail Elevation
1:200



2 South-West carpark Elevation
1:200



5 South-East Detail 2 Elevation
1:200

LEGEND

1. Signage Zone to future detail;
2. Dimensional Signage to future detail;
3. Galvanized steel frame and mesh panels;
4. Galvanized steel frame and perforated metal panels, painted finish to crest pattern;
5. Existing concrete panels wall to remain;
6. New concrete panel wall;
7. Painted steel panels or blades;
8. Painted steel panels;
9. Metal Louvers powdercoat finish;
10. Existing aluminum frame shopfront glazing to remain;
11. New aluminum framed shopfront glazing powdercoat finish;
12. Metal deck sheeting;
13. Glass balustrade;
14. Laminated timber soffit;
15. Exposed aggregate concrete paving;
16. Brick wall.

No.	Description	Date
A	DA Submission	13/03/2019
B	Approved DA	30/11/2019

SCOTTCARVER

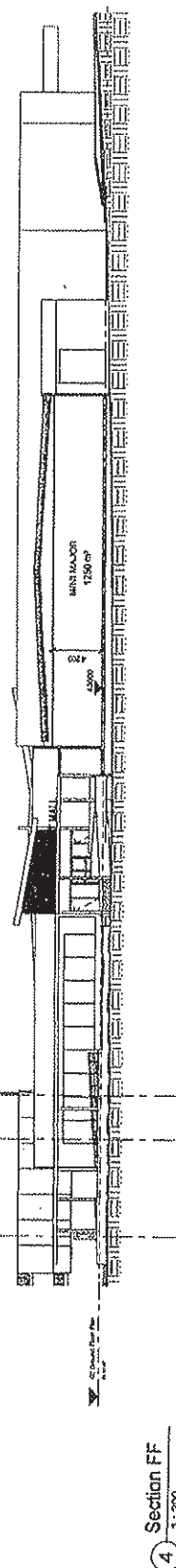
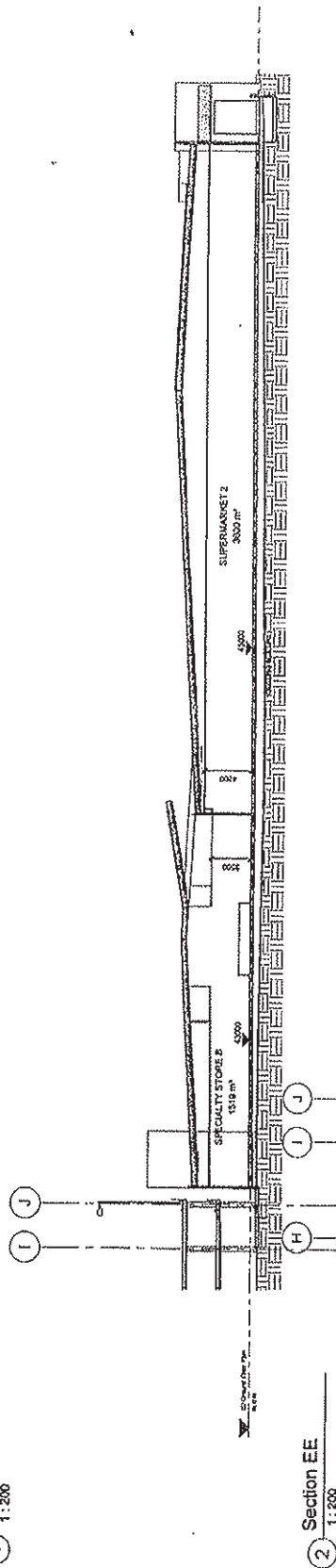
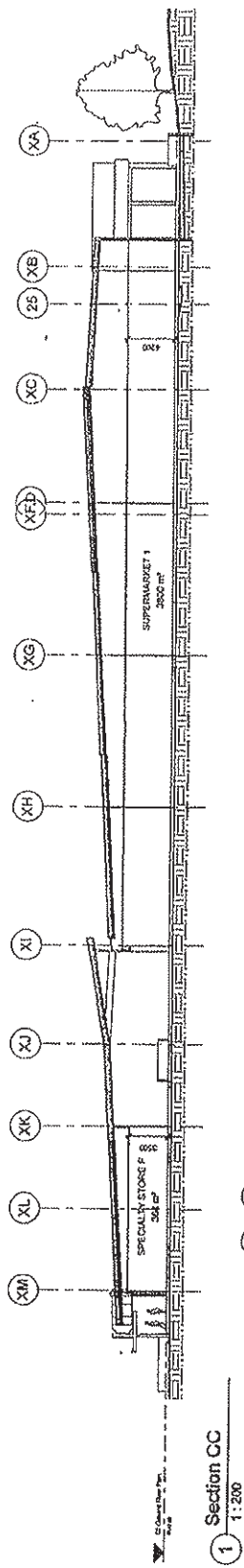
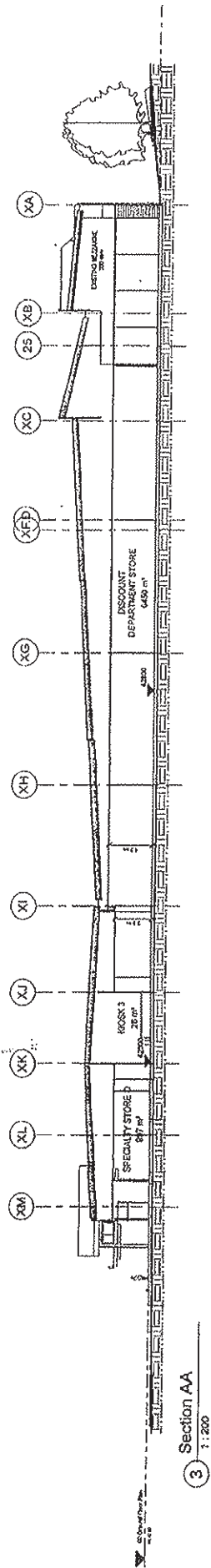
Gowrie Street Mail



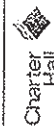
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Elevations

DA11 B



SCOTTCARVER



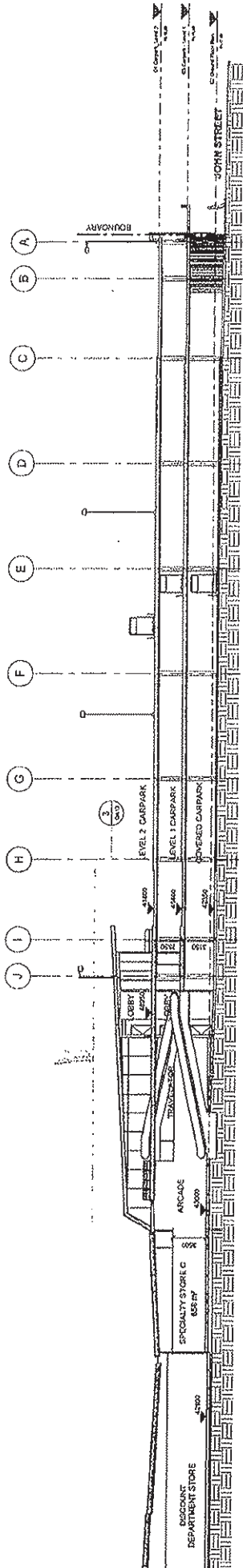
Gowrie Street Mall

1:200 @ A1

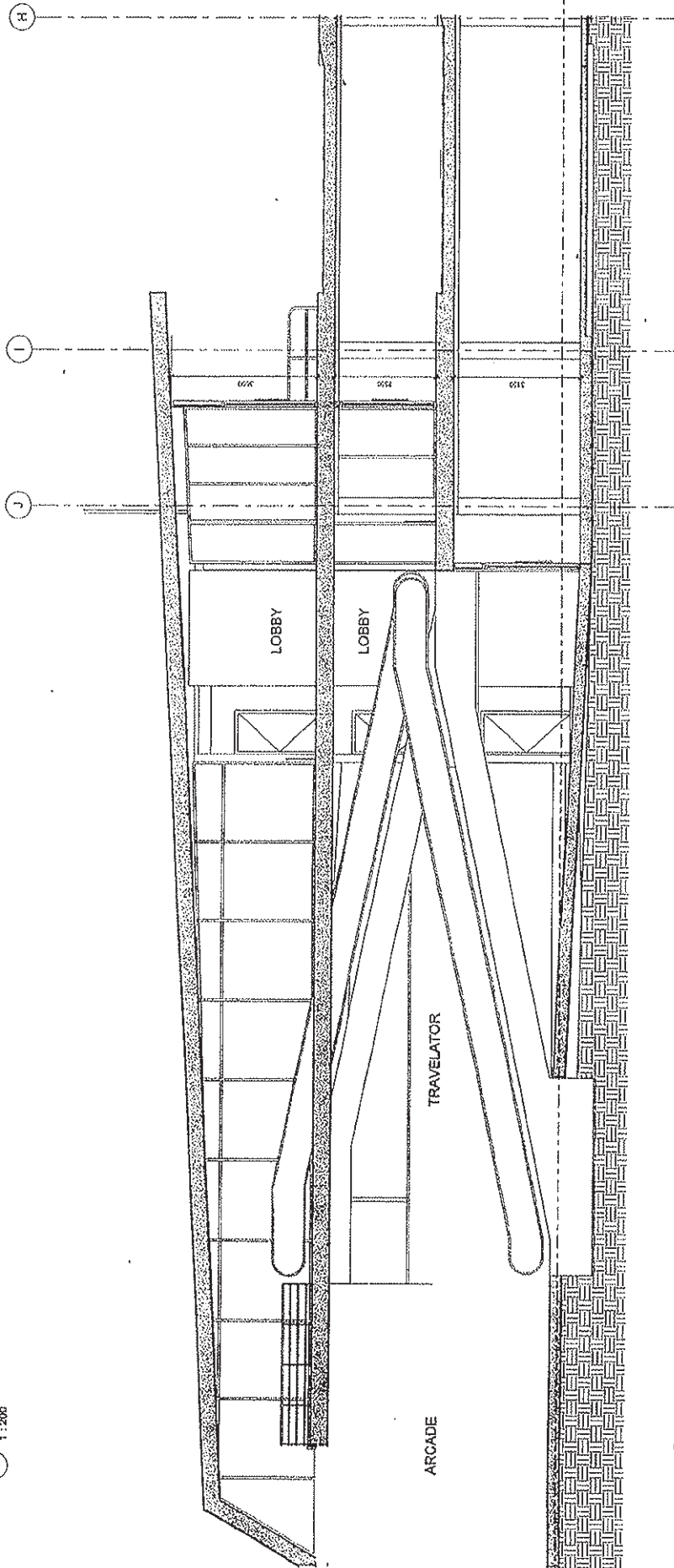


1:40000

Sections
DA12 A

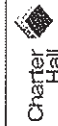


2 Section BB
1:200



3 Detail - Section BB
1:50

SCOTTCARVER



Gowrie Street Mall

As of 1/1/10

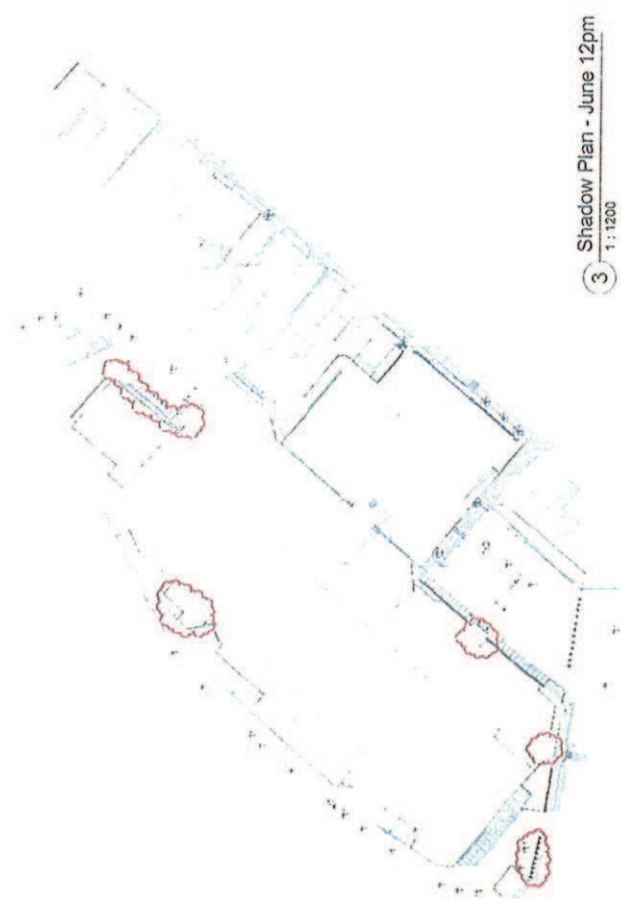
14/07/10

Sections

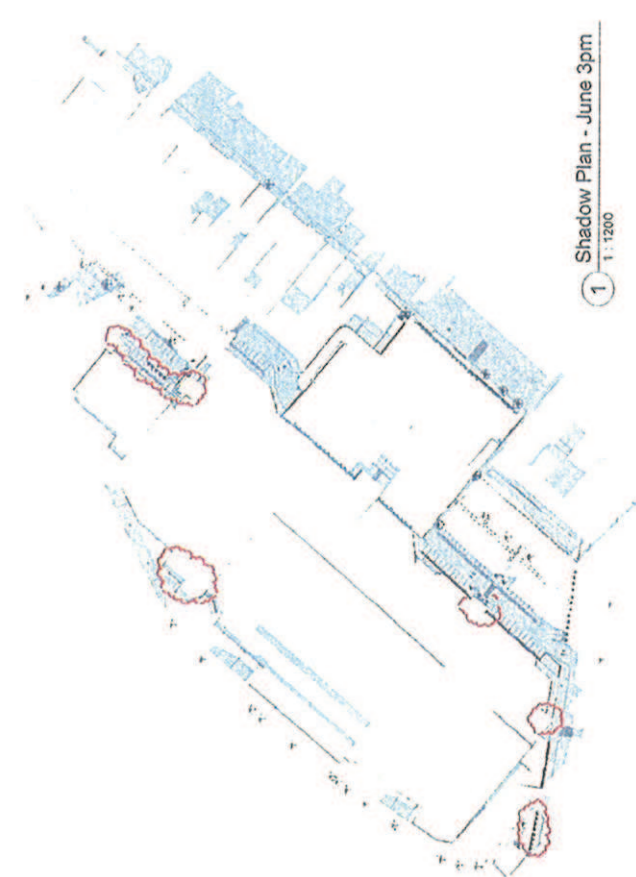
DA13 A



2 Shadow Plan - June 9am
1:1200



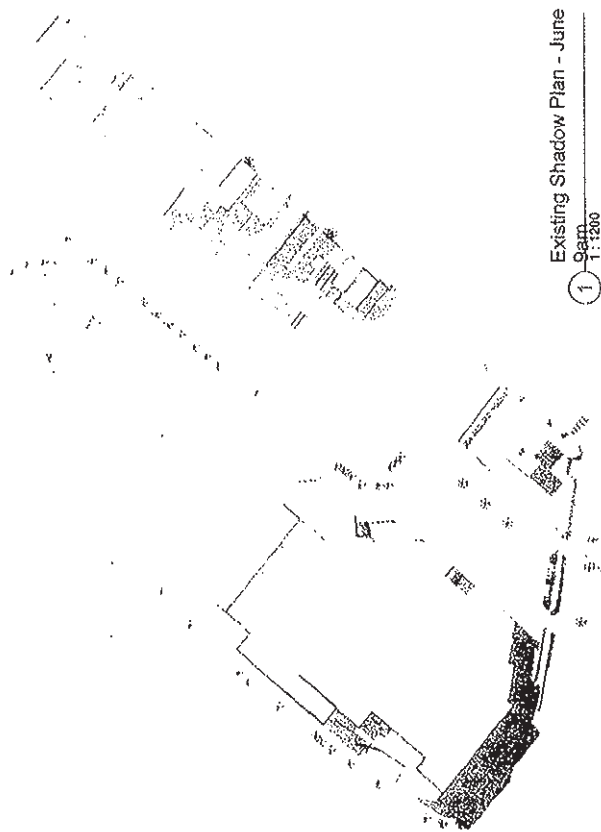
3 Shadow Plan - June 12pm
1:1200



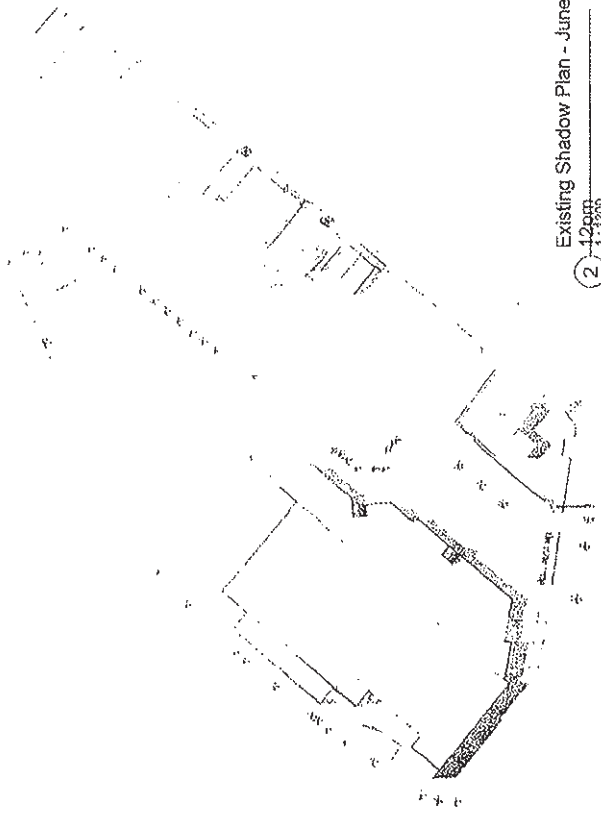
1 Shadow Plan - June 3pm
1:1200

RECEIVED
11 FEB 2011

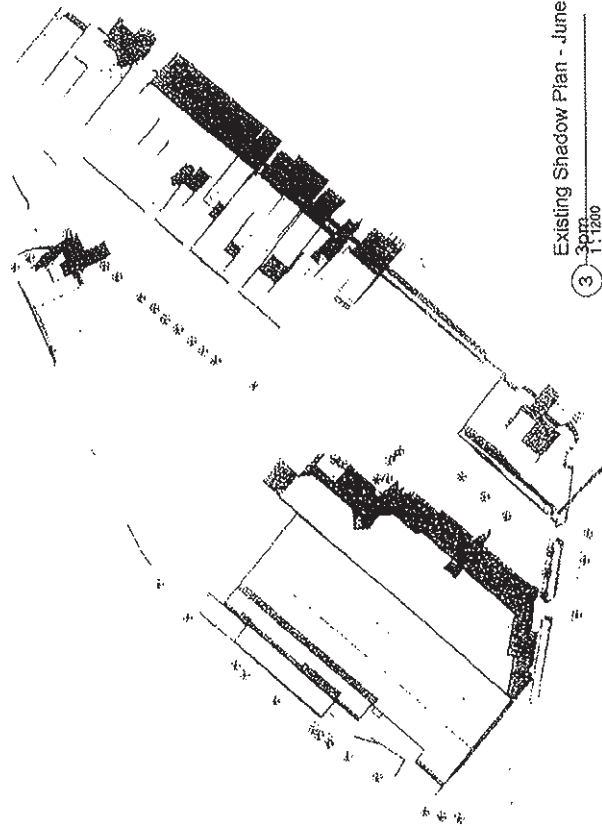
No.	Description	Date
A	DA Submission	13/07/2010
B	Amended DA - Issue 2	07/02/2011



Existing Shadow Plan - June
 1 0am 1:1200



Existing Shadow Plan - June
 2 12pm 1:1200

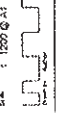


Existing Shadow Plan - June
 3 3pm 1:1200

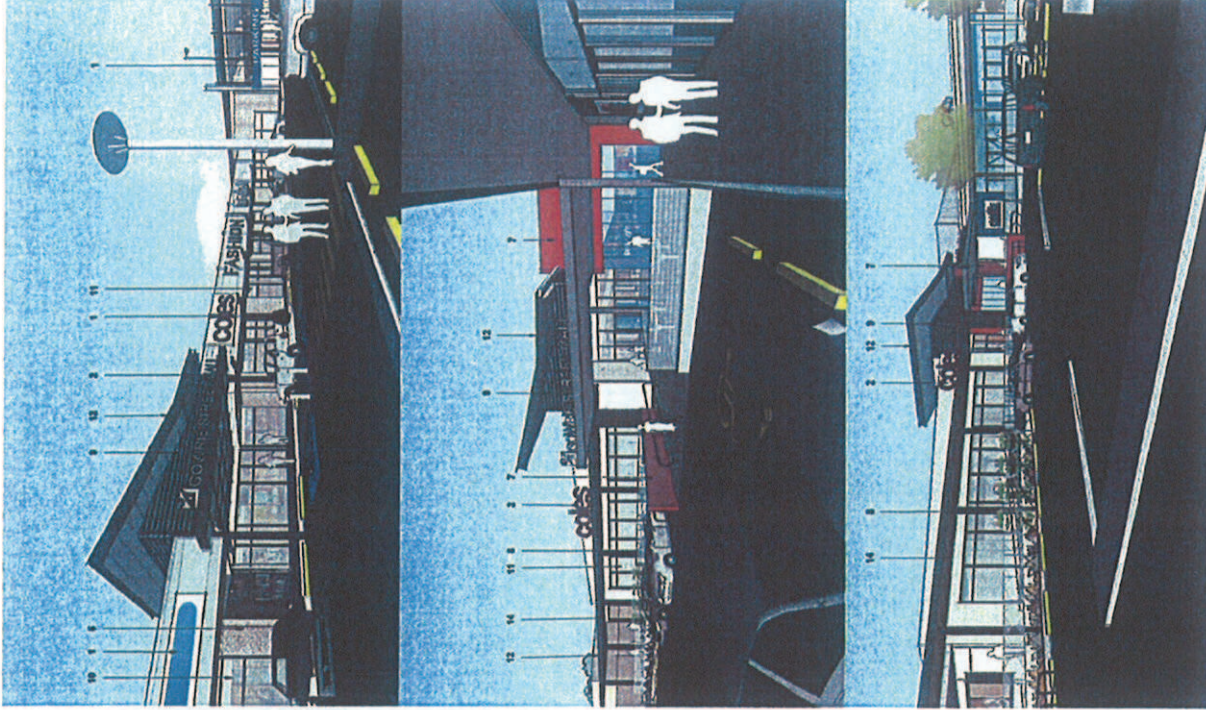
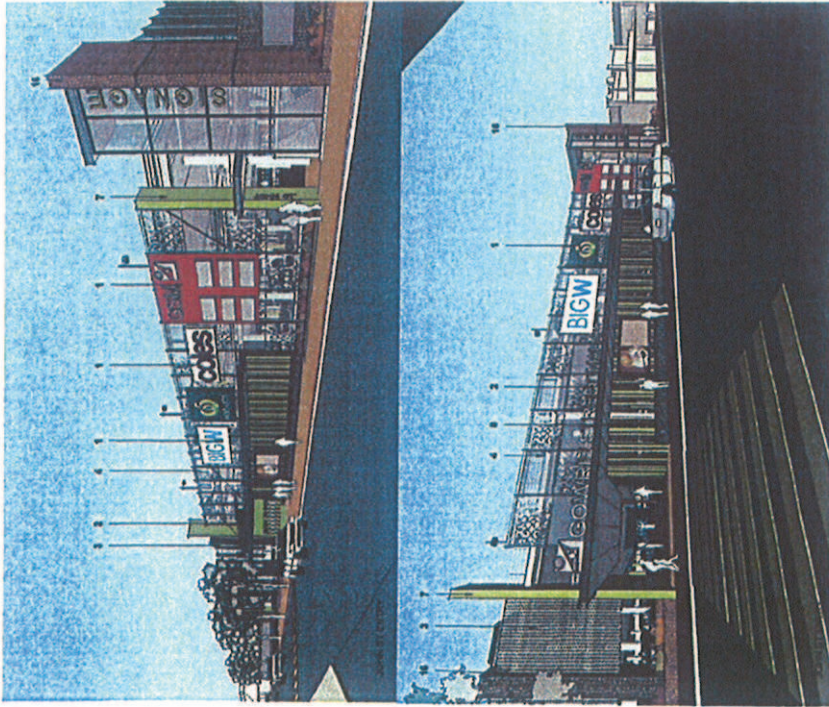
SCOTTCARVER



Gowrie Street Mail



Existing Shadow Diagrams
 14.0715 DA15 A



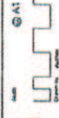
LEGEND

1. Signage Zone to future detail;
2. 3D dimensional Signage to future detail;
3. Galvanized steel frame and mesh panels;
4. Galvanized steel frame and perforated metal panels, painted finish to create pattern;
5. Existing concrete panels wall to remain;
6. New concrete panel wall;
7. Painted precast panels or blades;
8. Painted steel columns;
9. Metal Louvers powdercoat finish;
10. Existing aluminium frame shopfront glazing to remain;
11. New aluminium framed shopfront glazing powdercoat finish;
12. Metal deck sheeting;
13. Glass balustrade;
14. Laminated timber solid;
15. Exposed aggregate concrete paving;
16. Brick wall;

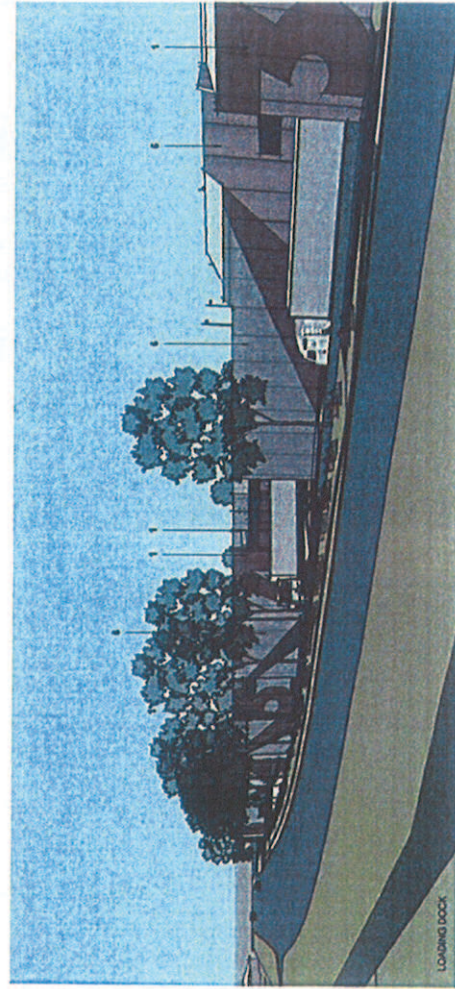
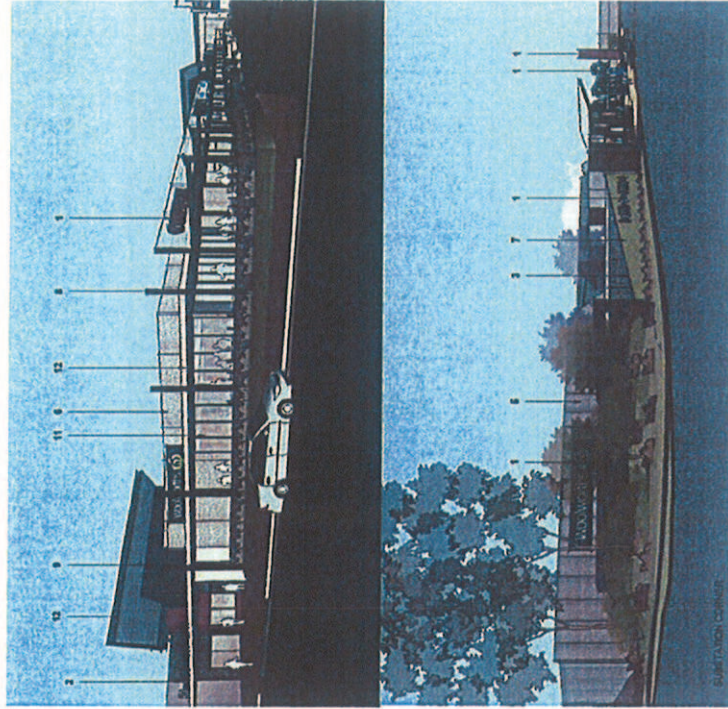
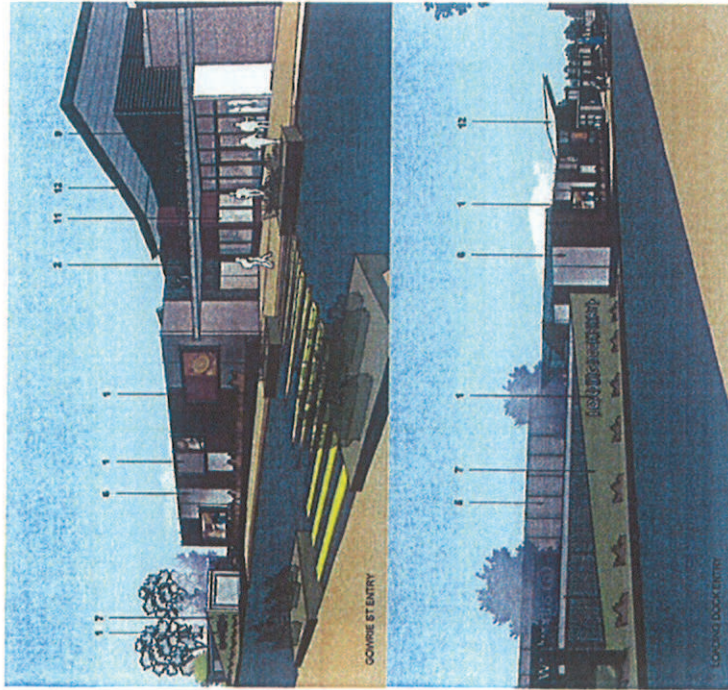
No.	Description	Date
A	DA Submission	13/07/2010
B	Amended DA	30/11/2010

SCOTTCARVER

Gowrie Street Mall



Materials Board
DA16-A B



SCOTTCARVER

Gowrie Street Mall



3011/16

Material Board
DA16-B B

No. Description Date
A DA Submission 13/07/2016
B Approved DA 30/11/2016

APPENDIX C – Referral Comments

Comments from External Agencies

- RTA
- NSW Police

Comments from Internal Departments

- Council Development Engineer
- Local Traffic Committee
- Disability Advisory Committee

402DA68; I
11/162, 11/165
BK



92

General Manager
Singleton Council
DX 7063
SINGLETON



Attention: Mr Frank Sullivan

RYAN AVENUE (MR 128): REFURBISHMENT AND EXTENSION OF GOWRIE STREET MALL, LOT 102 DP635677, 1 GOWRIE STREET, SINGLETON (DA249/2010)

Dear Mr Sullivan,

I refer to your letter and email dated 2 February 2011 (Your reference: DA249/2010), regarding the subject development application, forwarded to the Roads and Traffic Authority (RTA) for consideration under *State Environmental Planning Policy (Infrastructure) 2007* by the Hunter Regional Development Committee (HRDC).

As this project was previously considered by the HRDC at its meeting of 17 August 2010, it is considered that further review of the project by the HRDC is not warranted in this instance. However, on the basis of the information provided, the RTA has some concerns with the strategic traffic impact assessment by Traffix, dated January 2010 (should be 2011). The matters raised below should be addressed and forwarded to the RTA for further consideration, as they could substantially influence traffic generating outcomes and intersection / access requirements, including traffic signals and the classified road network:

- The trip generation adopted for the proposed development is based on an assumption made in a 2006 traffic assessment. Consideration should be given to undertaking surveys of the existing development to determine the validity of this assumption.
- The TIA incorrectly assumed a 15 per cent reduction in the number of trips generated as a result of 'linked trips'. The reduction should be applied to trips travelling past the site, not to trips entering / exiting the proposed development.
- It is understood most future residential growth is north of the site. Accordingly, it is expected there would be a strong bias in the trip distribution to and from the north, both for this and other surrounding developments. The 50% / 50% north / south split appears to be incorrect and should be revised to reflect actual directional splits.
- Figure 4 of the TIA shows 321 trips entering / leaving the local area. This is a significant reduction to the 376 trips suggested the proposed development would generate. Also, the diagram shows 29 trips turning left from Ryan Avenue (East) to John Street (South), which is unrelated to the site. Council should consider requesting further clarification of these matters.

REGISTERED AS
ELECTRONIC DOCUMENT

17 FEB 2011

File: DA249/2010

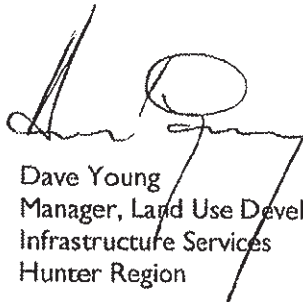
Doc No: 11/8171

Assignee: RTA

- The RTA would support a single lane circulating roundabout at the intersection of Ryan Avenue and Gowrie Street should Council decide to include this as a condition of consent. Council should ensure the roundabout is able to accommodate the design vehicle.
- The RTA would support a CHR / AUL type intersection or roundabout at the intersection of Ryan Avenue and Burns Lane should Council decide to include this as a condition of consent. The RTA would encourage Council not to support a seagull at this intersection on safety grounds.
- The service vehicle route appears not to meet design guide requirements both for vehicles entering and leaving the site. Turning path diagrams should be provided to demonstrate these movements are achievable.

Please contact me on 4924 0240 if you require further advice.

Yours sincerely,



Dave Young
Manager, Land Use Development
Infrastructure Services
Hunter Region

15 February 2011

11

**Hunter Regional
Development Committee**

402DA68
10/1393
SH

C/- RTA
Locked Bag 30
NEWCASTLE NSW 2300

Phone: (02) 4924 0240
Facsimile: (02) 4924 0342

General Manager
Singleton Council
DX 7063
SINGLETON

Attention: Mr Frank Sullivan

DA249/2010
10/1393/07
Ashley

**RYAN AVENUE (MR 128) – REFURBISHMENT AND EXTENSION OF GOWRIE STREET
MALL, SINGLETON**

Dear Mr Sullivan

I refer to your letter dated 30 July 2010 (Your reference: DA249/2010) regarding the subject development application.

The Hunter Regional Development Committee (HRDC) considered the application under the requirements of *State Environmental Planning Policy (Infrastructure) 2007*, at its meeting on 17 August 2010.

The Committee considered the proposed development and the Traffic Impact Assessment prepared by Traffix dated 2 July 2010.

- A previous development application was approved by Council in 2008, which increased the existing floor area from 12,220m² to 25,354m² and increased car parking provision from 365 to 984 spaces.
- The amendment proposed under this application increases the floor area from 12,220m² to 20,559m² and increases the car parking provision from 365 to 871 spaces


While the Committee has no objections to the proposed development it requires additional information and requests that the following matters be addressed and referred back to the RTA for further consideration by the HRDC:

- The Committee raised concerns regarding traffic impact that this development and other developments might have on the surrounding road network, particularly the classified regional road (Ryan Avenue).
- The traffic study was considered to be inadequate to assess the traffic / transport impacts of the proposed development on the surrounding road network. The study should be revised taking into account all known developments in the area and their cumulative and interactive impacts.

- The traffic study should be revised, to the satisfaction of the RTA and Council in accordance with the RTA's Guide to Traffic Generating Developments for all relevant intersections / accesses and include:
 - a. current traffic counts and 10 year traffic growth projections;
 - b. use of SIDRA or similar traffic model;
 - c. 95th percentile back of queue lengths on all legs;
 - d. delays and level of service on all legs;
 - e. road safety issues;
 - f. input / output data files.
- Appropriate intersection control measures should also be identified in the revised traffic study. Roundabouts should be considered at the Ryan Avenue intersections / accesses.
- The traffic study should also include an overall Traffic Management Plan for the entire precinct. This should be undertaken in consultation with Council.
- The option of restricting traffic movements to one way in Gowrie Street between the service station access the access to the mall car park should be considered. The majority of traffic to and from the shopping centre traffic should be directed onto Ryan Avenue and Burns Lane.
- Council raised the concern regarding buses using Gowrie Street. The partial closure of Gowrie Street / restriction of movements could require buses to change the route. This should be done in consultation with the relevant bus company
- Foot / cycle paths should be provided on site and connected to external networks.
- Safe and secure bicycle parking should be provided on-site.
- Car parking should be to Council requirements.
- Disabled car parking spaces should be provided to Council requirements.
- A taxi drop off/pick up area should be provided close to the main access to the shopping centre.
- Turning templates should be provided for all service vehicles for all intersections and accesses.
- A truck management plan, including hours of operation and travel routes, should be prepared and submitted to Council / RTA for approval.
- Details of the waste pick up area should be provided.
- Emergency vehicle access should be provided to / from the site.
- On-site and street lighting should be provided to Australian standards.
- A construction management plan should be prepared and submitted to Council / RTA for approval.

Please contact me on (02) 4924 0240 if you require further advice.

Yours sincerely

A handwritten signature in black ink, appearing to read 'Dave Young', is written over the typed name and title.

Dave Young
Chairperson
Hunter Regional Development Committee

15 September 2010

402DA68;1
10/1393
SH



General Manager
Singleton Council
DX 7063
SINGLETON

Attention: Frank Sullivan

RYAN AVENUE (MR 128) – REFURBISHMENT AND EXTENSION OF GOWRIE STREET MALL, SINGLETON

Dear Mr Sullivan

I refer to your letter dated 30 July 2010 (Your reference: DA249/2010) regarding the subject development application forwarded to the RTA for consideration at the Hunter Regional Development Committee (HRDC). Due to the impact on the classified road network, the RTA considers it appropriate to reply separately advising of its comments concerning the above-mentioned development application. Some of these comments may reflect those made by the HRDC.

The RTA's primary interests are in the road network, traffic and broader transport issues, particularly in relation to the efficiency and safety of the classified road system, the security of property assets and the integration of land use and transport.

In accordance with the *Roads Act, 1993*, the RTA has powers in relation to road work, traffic control facilities and other structures on the classified road network. As Ryan Avenue (MR 128) is a classified (regional) road RTA concurrence is required for connections to that road with Council consent. Council is the roads authority for that road and all other public roads in the area.

The RTA requires additional information and requests that the following matters be addressed and referred back to the RTA for further consideration:

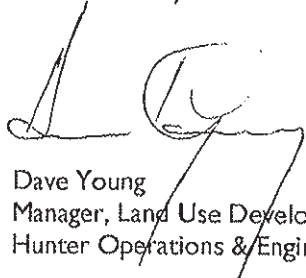
- The RTA has concerns with the traffic and road safety impacts that this development would have on Ryan Avenue. The traffic study was considered to be inadequate to assess the traffic / transport impacts of the proposed development on the road network. The study shall be revised taking into account all known developments in the area and their cumulative and interactive impacts.

Roads and Traffic Authority

- The traffic study shall be revised, to the satisfaction of the RTA and Council in accordance with the *RTA's Guide to Traffic Generating Developments* for all relevant intersections / accesses and include:
 - a. current traffic counts and 10 year traffic growth projections;
 - b. use of SIDRA or similar traffic model;
 - c. 95th percentile back of queue lengths on all legs;
 - d. delays and level of service on all legs;
 - e. road safety issues;
 - f. input / output data files.
- Appropriate intersection control measures should also be identified in the revised traffic study. Roundabouts should be considered at the Ryan Avenue intersections / accesses.
- The traffic study should include an overall Traffic Management Plan for the entire precinct. This should be undertaken in consultation with Council.
- Turning templates should be provided for all service vehicles for all intersections and accesses.
- A truck management plan, including hours of operation and travel routes, should be prepared and submitted to Council / RTA for approval.
- Street lighting should be provided to Australian standards.
- A construction management plan should be prepared and submitted to Council / RTA for approval.

Please contact me on (02) 4924 0240 if you require further advice.

Yours sincerely



Dave Young
 Manager, Land Use Development
 Hunter Operations & Engineering Services

15 September 2010

65

Singleton Council
20 DEC 2010
File Ref.....



NSW Police Force
www.police.nsw.gov.au

Singleton Council
P.O. Box 314
Singleton

13 December, 2010

Attention: Town Planner, Mrs A Clark.

Refurbishment and Extension of Gowrie Street Mall, Singleton.
LOT: 102 DP: 635677, 1 Gowrie Street SINGLETON 2330.

In September, 2010 the Hunter Valley Command received a development application for the extension and refurbishment of the Gowrie Street Mall in Singleton.

In April 2001 the NSW Minister for Planning introduced Crime Prevention Guidelines to Section 79C of the Environmental Planning and Assessment Act, 1979. These guidelines require consent authorities to ensure that development provides safety and security to users and the community. 'If a development presents a crime risk, the guidelines can be used to justify modification of the development to minimize crime risk, or, refusal of the development on the grounds that crime risk cannot be appropriately minimised'.

Crime Prevention Through Environmental Design (CPTED)

Crime Prevention Through Environmental Design (CPTED) is a crime prevention strategy that focuses on the planning, design and structure of cities and neighbourhoods. It reduces opportunities for crime by using design and place management principles that reduce the likelihood of essential crime ingredients from intersecting in time and space.

REGISTERED AS
ELECTRONIC DOCUMENT
20 DEC 2010
File: DA249/2010
Doc No: 10/56955
Assigned: Rita

Hunter Valley Local Area Command

Predatory offenders often make cost-benefit assessments of potential victims and locations before committing crime. CPTED aims to create the reality (or perception) that the costs of committing crime are greater than the likely benefits. This is achieved by creating environmental and social conditions that:

- Maximize risk to offenders (increasing the likelihood of detection, challenge and apprehension).
- Maximise the effort required to commit crime (increasing the time, energy and resources required to commit crime)
- Minimise the actual and perceived benefits of crime (removing, minimizing or concealing crime attractors and rewards) and
- Minimise excuse making opportunities (removing conditions that encourage/facilitate rationalization of inappropriate behaviour)

CPTED employs four key strategies. These are surveillance, access control, territorial re-enforcement and space/activity management.

The application and attached plans have been viewed. The Town Planning Report does outline briefly some design applications (6.5.2) that take into consideration the principals of CPTED. These are important and have been applied well within the development. The following comments are made regarding the development application and plans provided.

SURVEILLANCE:

Natural surveillance is achieved when normal space users can see and be seen by others. This highlights the importance of building layout, orientation and location; the strategic use of design; landscaping and lighting. *Natural surveillance* is a by-product of well-planned, well-designed and well-used space. *Technical/mechanical Surveillance* is achieved through mechanical/electronic measures such as CCTV, help points and mirrored building panels. *Technical/mechanical surveillance* is commonly used as a 'patch' to supervise isolated, higher risk locations. *Formal (or Organised) Surveillance* is achieved through the tactical positioning of guardians. An example would be the use of on-site supervisors at higher risk locations.

General Comments:

- Buildings facing 'outward' towards public and semi public areas provide natural surveillance and informal supervision (eyes on the street).
- Entry points should be designed so as to maximize surveillance opportunities to and from these areas from both inside as well as outside.
- The placement and orientation of common entry areas should maximize opportunities for natural supervision by staff and other guardians.
- Laminated glass walls and windows facilitate supervision of common entry areas.

Recommended Conditions of Consent:

- There is no mention in the application regarding surveillance cameras within and outside the development. Surveillance equipment can enhance the physical security of the site and assist in the identification of people involved in anti-social or criminal behaviour. The loading dock areas should be monitored due to their location to the rear of the development and away from areas where informal supervision.
- CCTV cameras should be utilised in the centre and monitor access points.

LIGHTING

There is a proven correlation between poor lighting, fear of crime, the avoidance of public places and crime opportunity (Painter, 1997). Good lighting can assist in increasing the usage of an area. There is no information with the plans, which were reviewed to indicate the lighting proposals for the development. The report states that the building designs uses natural lighting where possible and artificial lighting used when required.

General Comments:

- Lighting should be designed to the Australian and New Zealand Lighting Standards.
- Australia and New Zealand Lighting Standard 1158.1 – Pedestrian, requires lighting engineers and designers to consider crime risk and fear when selecting lamps and lighting levels.

Recommended Conditions of Consent:

- A lighting maintenance policy needs to be established for the development.
- Install security lighting in and around the businesses, particularly over entry/exit points to create an even distribution of light with no glare, e.g. sensor lighting, floodlighting.

NB: Consider installing sensor lighting, which is cost effective as it only, activates when movement is detected within the zone.

- Special attention to be paid to lighting for pedestrian pathways and movement predictors (to permit facial recognition of person at 15 metres). Areas adjoining pathways should be illuminated to avoid opportunities for concealment/entrapment. High quality, vandal resistant lamps are less likely to require replacement or maintenance.
- To increase surveillance, reduce fear, in the covered car park, and maximise lighting, paint the ceilings and walls white, this helps reflect light.

TERRITORIAL RE-ENFORCEMENT.

Criminals rarely commit crime in areas where the risk of detection and challenge are high. People who have guardianship or ownership of areas are more likely to provide effective supervision and to intervene in crime than passing strangers. Effective guardians are often ordinary people who are spatially 'connected' to a place and feel an association with, or responsibility for it. *Territorial Re-enforcement* uses actual and symbolic boundary markers, spatial legibility and environmental cues to 'connect' people with space, to encourage communal responsibility for public areas and facilities, and to communicate to people where they should/not be and what activities are appropriate.

General Comments

- The boundaries of the development are well defined.
- Loading dock area should be clearly define the territorial space of the development and well secured.
- All access points should be closed and locked when not in use and regularly maintained to assist with protection of property.
- Effective signage and directions will proved guidance to visitors/customers in locating main areas and keep them from restricted areas.
- Signs can also assist in controlling activities and movements throughout the premises.
Eg, Skateboarding prohibited.

Recommended Conditions of Consent:

Environment maintenance

- Clean and well maintained areas send clear message to define space use.
- Vandalism and graffiti removal plan should be in place. The use of graffiti resistant paint and a quick removal policy.
- Well placed garbage bins though out the centre encourages patrons not to litter. Cleaners moving though out the centre provide good surveillance and guardianship of areas.
- Effective signage and directions will provide guidance to visitors/customers in locating main areas and keep them away from restricted areas.

- Signs can also assist in controlling activities and movements throughout the premises.
- The centre currently has signage. After improvements the signs should be reviewed to ensure that they apply. Confusion resulting from vague entry design can legitimise exploration, trespassing and excuse making by opportunistic criminals. Entries should be legible and inviting.
- Signage also needs to be provided at entry/exit points and throughout the development to assist users and warn intruders they will be prosecuted.
- Landscaping should be well maintained with low vegetation to keep it open, maintain a line of sight and not provide entrapment/ concealment areas.
- Monitoring unauthorised use of the car parking area by skateboarders, scooters and bicycles users. Other covered car parks within the Singleton CBD have been used as an area to practise tricks and young people will congregate. In some instances this resulted in malicious damage (graffiti) offences.

SPACE/ ACTIVITY MANAGEMENT.

Space/Activity Management strategies are an important way to develop and maintain natural community control. Space management involves the formal supervision, control and care of the development. All space, even well planned and well designed areas need to be effectively used and maintained to maximise community safety. Places that are infrequently used are commonly abused. There is high correlation between urban decay, fear of crime and avoidance behaviour.

General Comments:

- The front of the centre and entry/exit points all face the car parks. Most activity will be patrons accessing the stores inside and with the smaller shops closing around 5.30pm there will be limited other street activity or people moving through the development to access other venues.
- The car park should be clearly defined and any unauthorised use quickly stopped eg skateboard riding.
- Trolleys should be gathered and secured when not in use. From plans provided there are no trolley bays identified on the plans other than a trolley storage area. It is envisaged that customers will take the trolleys to their vehicles to unload and well placed trolley bays encourage proper disposal after use.
- There is no identified area for bicycles to be parked. Bicycle parking should be placed within view of capable guardians.

- There is no taxi bay or drop down or pick up zone for Community vehicles catering for the elderly.
- No designated parking zones for emergency vehicles.

Recommended Conditions of Consent:

- Designated zone for drop off/pick up for community vehicles and taxi's.
- A two car spaces near the front of the development be marked for emergency vehicles only.

ACCESS CONTROL

Access control treatments restrict, channel and encourage people and vehicles into, out of and around the development. Way-finding, desire-lines and formal/informal routes are important crime prevention considerations.

Access control is used to increase the time and effort required to commit crime and to increase the risk to criminals. *Natural access control* includes the tactical use of landforms and waterways features, design measures including building configuration; formal and informal pathways, landscaping, fencing and gardens.

Technical/Mechanical access control includes the employment of security hardware and *Formal (or Organised) access control* includes on-site guardians such as employed security officers.

General Comments

- There is again little/ no information to indicate the access control treatments in and around the development.
- Fire exit doors to the development should also be fitted with single cylinder locksets (Australia and New Zealand Standard – Lock Sets) to restrict unauthorized access to the development.
- Access control should be set in place to exclude unauthorized access to restricted areas, particularly to the loading docks.
- There is no indication of what control measures are taken regarding access from the first and second floor car park. A potential opportunity exists for offenders to gain access to businesses in the development if access is not controlled or of a high standard.
- No indication of access controls for fire stairs in car park.

Recommended Conditions of Consent:

- The main entry/exit points for this development should be fitted with single cylinder locksets (Australia and New Zealand Standards – Locksets), which comply with the Building Code of Australia.
- Fire exit doors to the development should also be fitted with single cylinder locksets (Australia and New Zealand Standard – Lock Sets) to restrict unauthorized access to the development.
- Bollards or barriers can be installed to reduce the opportunities for ram raid attacks.
- Businesses retaining money on premises should have a safe, designed and installed to the Australian Standards to provide additional security to money and other valuables.
- To enhance the security of the business, a monitored intruder alarm system is recommended.
 - Consider incorporating a duress facility into the system for those businesses like the liquor store and supermarket, which are open after normal trading hours, to enable staff to activate the system manually in the event of an emergency, such as a robbery.
- Vehicle ramps should be corrugated to reduce opportunities for use by skate boarders, skaters or bike riders and increase vehicle traction.
- Access to the loading dock needs to be controlled and restricted to all unauthorized persons. As there is minimal surveillance to this area, proper security needs to be considered.
 - Security doors should be fitted to the loading dock's main vehicle entry/exit point to restrict unauthorized access
 - Having electronic access control equipment fitted to these areas to enhance physical security.

Conclusion

The New South Wales Police have a vital interest in ensuring the safety of members of the community and their property. By using the recommendations contained in this evaluation, any person who does so acknowledges that:

1. It is not possible to make areas evaluated by the NSW Police absolutely safe for members of the community or their property
2. It is based upon the information provided to the NSW Police at the time the evaluation was made,
3. The evaluation is a confidential document and is for use by the consent authority or organizations referred to on page 1 only,
4. The contents of this evaluation are not to be copied or circulated otherwise than for the purposes of the consent authority or organization referred to on page 1.

The NSW Police hopes that by using the recommendations contained in this document, criminal activity will be reduced and the safety of members of the community and their property will be increased. However, it does not guarantee that all risks have been identified, or that the area evaluated will be free from criminal activity if its recommendations are followed.

We would like to thank you for the opportunity of inspecting the plans for this development and should you require further information on the subjects mentioned within this report feel free to contact Senior Constable Sheree Gray, Crime Prevention Officer, Hunter Valley LAC, Phone 6542-6999.



T. King
Inspector
Crime Manager
Hunter Valley LAC



Civic Centre
Queen Street
SINGLETON NSW 2330
PO Box 314
DX 7063
Phone: 02 6578 7290
Fax: 02 6572 4197

Development Engineer Referral Response

To:	Alison Clark
From:	Frank Sullivan
Date:	16 February 2011
File:	DA249/2010 – Gowrie St Mall Extension.

DA Only <input checked="" type="checkbox"/>	DA/CC <input type="checkbox"/>	DA/SA <input type="checkbox"/>
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Stormwater Management/Drainage		
Applicable 		

The site is crisscrossed with Council major stormwater lines which drains parts of John Street, Burns Ave, the Council carpark and commercial centre north of Burns Lane and the subject site. The line crosses Ryan Avenue, is under the levee bank and discharges at a large headwall infrastructure in Cook Park/Rose Park and incorporates flood control gates. It is a significant stormwater system and will require the ability to access for maintenance and repairs. Council's preference is that this stormwater line is maintained clear of any building structure which will require relocation. Furthermore, the subject development will be required to incorporate water quality and quantity devices that meet Council's DCP requirements and a large GPT that is accessible for maintenance.

The site itself is inundated by the 1:100 year flood event and is to be designed to withstand the forces of flood waters as well as using building materials resistant to water inundation.

Utilities		
Applicable	Preliminary Assessment Undertaken	Further Information likely to be required prior to issue of Construction Certificate
	Complete Assessment Undertaken	
<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
Comments		

Roads are dealt with hereunder. All utilities are available to the site but may require amplification. The existing sewer reticulation system traverses the site generally parallel to John Street and currently services the Caltex service station on the corner of John and Gowrie Streets, the old NRMA building and the adjoining building, the existing mall and the Woolworths Caltex service station. The sewer line is an old vitreous clay pipe and will generally require replacement under the multi storey car park. Services are to be maintained to all existing customers. Major water mains exist in Gowrie and John Street and the applicant is to ensure that full fire service flows are available to the site. Council will also require an estimate of the number of water services required.

Roads/Driveway/Access/Traffic		
Applicable	Preliminary Assessment Undertaken	Further Information likely to be required prior to issue of Construction Certificate
	Complete Assessment Undertaken	
<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>

Car Parking:

RTA Traffic generating development indicates the minimum recommended level of off-street parking for a retail shopping centre of the subject size to be 4.3 car parking spaces per 100m² of GLFA. It further suggests that as a guide 75% of GFA equates to GLFA. This provides a GLFA of 18471m² for the subject development which would require 794 car parking spaces. Council's DCP is silent on the number of car parking spaces required for a retail shopping centre and the traffic study has adopted the DCP's car parking for shops quoted as 1 car parking space per 30m² of GFA which would require 821 car parking spaces. The previously approved

development for this site which approved 25354m² of GFA also approved 968 car parking spaces (1 space per 26m² GFA). To further compare the car parking spaces if the number of car parking spaces per sq meter of GFA of the existing approved DA is calculated (1 space/ 26m²) and applied as a pro rata rate to GFA for the DA currently with Council for assessment, 947 spaces is required.

As a matter of history, Council was the previous owner of the land on which Gowrie Street Mall sits and a condition of the sale by way of an 88B instrument was that 120 car parking spaces were to be kept and maintained as a public carpark to take account of the public car parking spaces required for the occupants and customers of those existing businesses that fronted John Street but had an historical deficiency of car parking spaces. This 120 spaces is in addition to any car parking spaces required by the retail development now occupied by Gowrie Street Mall.

Tabulated Results of car parking spaces required for Subject Development.

RTA	DCP	Pro Rata rate	Public Carparking	Total required
794	821	947 (includes the 120 public)	120	914 (RTA) 941 (DCP)

In summary, and as a minimum requirement, 914 car parking spaces should be provided as part of the development.

Vehicle Traffic Network.

An initial traffic impact assessment dated July 2010 was presented to Council as part of the statement of environmental effects. Council and the Regional Development Committee (RTA convened) agreed that the traffic assessment was inadequate as it failed to address the broader traffic issues that together with the subject development potentially could impact significantly in the medium to long term on the traffic network in the CBD. The applicants were requested to increase the scope of the traffic impact assessment and in this regard Council have agreed to partly fund the assessment because it could be seen to benefit Council in its broader strategic traffic planning for the CBD.

The expanded traffic study January 2011, was lodged with Council and forwarded to the Regional Development Committee for comment. There are a number of basic mistakes in the study and the effect of these mistakes on the final SIDRA analysis cannot be determined without an in-depth analysis of the SIDRA input. The width

of Ryan Avenue is overstated in the report as 14 wide whereas it is 12.9m, Gowrie Street is stated as 16m whereas it is 11m, the right turn storage lane in John Street to turn into York Street has not been acknowledged and it would appear that peak generation rates and trip generation rates have been underestimated without adequate justification. However it is considered that the conclusions presented in the assessment have merit and acknowledge that the Level of Service at the intersection of Gowrie and Ryan Avenue is not totally relevant and of more relevance is that the safety of the intersection should be improved because of the poor sight distance and similar safety considerations will require the upgrade of Ryan Avenue/Burns Lane intersection.

In the final analysis it would be imprudent and irresponsible to approve the development based on the unacceptable effect to the safety of the road network without improvements to the Gowrie St/Ryan Ave intersection, Ryan Ave/Burns Lane intersection and some other minor improvements to the road network. In summary, the following improvements to the road network will be required prior to an Occupation Certificate:

- Roundabout or traffic lights at Ryan Ave/Gowrie Street intersection.
- A channelised intersection treatment at Burns Lane/Ryan Avenue with the inclusion of a cycle refuge.
- Burns Lane to remain as a two way street.
- The intersection of Gowrie Street with John Street to remain as a no right turn from Gowrie St into John St.
- Lot 112 DPI005149 shown as Laneway is to be constructed to include a 1.5m concrete footpath, 2.6 parking lane, 2 x 3.5m travel lanes as a minimum and provide the ability for service vehicles delivering to the rear of the properties with frontage to John Street to turn around. Access through the Burns Lane carpark is the most feasible turning area and the swept path of 12.5m vehicles should be tested to ensure that it is achievable.

The loss of vehicle parking in Ryan Avenue as a consequence of the proposed channelised intersection with Burns Lane together with the relocated exit of the service vehicle lane is considerable. This loss is particularly critical of an afternoon when football practice is in progress at Cooks Park at which time most of the length of Ryan Avenue is occupied with vehicles parked at the kerbside. Cooks Park is covered by a masterplan which proposes additional parking and this additional parking becomes critical once there is loss of car parking on-street in Ryan Avenue. Accordingly there is a close if not direct nexus with the loss of parking in Ryan Avenue and the need to provide additional parking in Cooks Park and the development will be conditioned to make a contribution to this carparking.

Service Vehicles.

The entry and exit of service vehicles to the site have been relocated from their existing position to better accommodate the extension of the buildings. Swept path

diagrams have been submitted for all service vehicles other than the exit onto Ryan Ave. The swept path diagram off Gowrie Street does not make allowance for the proposed roundabout at the intersection with Ryan Avenue but initial investigation demonstrates that it is possible depending on the configuration of the roundabout. Of greater concern is the exit service lane onto Ryan Avenue. It is not possible to turn left out of the service lane onto Ryan Avenue leaving the only possible turn as a turn right at an awkward skewed angle. This will have the effect of directing all service vehicles that leave the site to the traffic lights at John/Hunter/Ryan Avenue then to the Campbell Street intersection with the New England Highway.

Additionally, the location of the Gowrie Street service vehicle entrance due to its closer proximity to the intersection of Gowrie St and Ryan Avenue will require considerable screwing of service vehicles to negotiate the turn into the service entrance and there will be at least a doubling of the number of service vehicles. Gowrie Street is only a two coat bitumen seal and the wearing course will easily strip under this vehicle movement and has already been patched on many occasions. Consequently it will be a condition of development that the developer reconstruct Gowrie Street from the Ryan Avenue intersection to the service vehicle entrance with a wearing course of a minimum 50mm AC14 subject to a pavement design.

Pedestrian Traffic Network.

Pedestrian/cycleway connectivity to the development is paramount and the safety, ease of access and connectivity to existing pedestrian/cycleway thoroughfares is mandatory. The traffic assessment and the Statement of Environmental effects proposes a number of pedestrian crossings but there is no supporting information that demonstrates that the warrants are met to require marked crossings. Furthermore, the proposed crossing of John Street in the vicinity of York Street ignores the fact that there are three lanes of traffic carrying out complicated maneuvers hence the points of conflict are unacceptable. If the crossing is to remain at this location it will require widening of John Street at this location and the inclusion of a cycleway refuge. Alternatively and a better option is to relocate this crossing to a position north of York Street. This crossing is to have kerb blisters to provide additional standing spaces for pedestrians about to cross and to have a calming effect on through vehicle traffic.

To further improve the pedestrian network and to comply with Council's adopted Singleton CBD Caretaker Service Area, both John Street and Gowrie Street frontages are to be paved with Council approved paver type and colour.

As previously mentioned a 1.5m wide concrete footpath will be required along the laneway at the rear of the John Street shops to provide connectivity from John Street/Burns Lane along the laneway to the proposed internal pedestrian crossing.

The proposed pedestrian crossing of Gowrie Street is acceptable and is required and again will require kerb blisters for the same reasons as in John Street.

With regard to the main entrance to the development on John Street, the proposal is totally unacceptable and potentially dangerous with regard to pedestrian traffic movement along John Street. The proposal shows covered seating at the entrance which will encourage a pedestrian meeting place which is likely to occur with or without the seating. In fact, the statement of environmental effects highlights the fact that they wish to activate John Street by providing a focal point at the entrance. This is a good outcome but, unfortunately, the existing width of the footpath is completely inadequate to safely provide for this. It is at this location that vehicle traffic is at its most congested on John Street with complicated maneuvers into York Street and through traffic on John Street. Drivers are not paying attention to pedestrians. The potential conflict of pedestrian traffic and vehicle traffic is high. An adequately sized vestibule is required to separate pedestrian traffic gathering to enter the shopping centre from pedestrian traffic walking along John Street.

Pedestrian/cycleway connectivity is required from the cycleway located in Cooks Park to cross Ryan Avenue and link to the subject development. The existing cycleway in Cooks Park connects to Ryan Avenue in the vicinity of the Burns Lane intersection. Crossing of Ryan Avenue at this location is complicated by the Burns Lane intersection and the proximity of the service vehicle exit onto Ryan Avenue. A pedestrian/cycleway refuge is to be incorporated in the crossing but a marked crossing is unlikely but should be tested against the warrants required for marked crossings. A similar crossing will be required at the Gowrie Street/John Street intersection to cross John Street.

It will also be a requirement that all street crossings and intersections have lighting in accordance with AS1158.

Landscaping		
Applicable	Preliminary Assessment Undertaken	Further Information likely to be required prior to issue of Construction Certificate
	Complete Assessment Undertaken	
<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
Comments		

A concept plan for landscaping has been lodged with the application but due to changes of other plans the landscape plan is no longer consistent with those plans in reflecting the current position with regard to bus parking.

The relocation of the service road closer to the boundary along Ryan Avenue and the works associated with that road together with the proposed substation chamber and extension of the staff car park is likely to remove most if not all of the trees along Ryan Avenue. It also means that the only viable planting space for landscaping along Ryan Avenue is within Council road reserve. Whilst this is not uncommon in residential areas it is not the usual practice in commercial and industrial areas and some form of agreement may be required to ensure that the applicant has care and maintenance of the landscape along Ryan Avenue. Furthermore, the unfavorable batter slopes created along Ryan Avenue as a result of the service road relocation may require retainer walls to give enough planting space and pedestrian walking space along the grass verge of Ryan Avenue.

The proposed planting at the entrance to the walkway off John Street shown on the amended plans, which are now inconsistent with the concept landscape plan, are poorly conceived and will ultimately end up as a receptacle for rubbish and cigarette butts, poorly maintained plants exasperated by the fact that they are removed in distance and context from the main buildings basically out of sight out of mind.

Insufficient landscaping consideration has been given to the development and the DA will be conditioned to require a detailed landscape design particularly of Ryan Avenue, Gowrie Street and John Street but also to provide shade and visual amenity in the expanse of carpark.

Earthworks/Geotechnical		
Applicable	Preliminary Assessment Undertaken	Further Information likely to be required prior to issue of Construction Certificate
	Complete Assessment Undertaken	
✓	✓	✓
Comments		

Earthworks other than in a public road will be conditioned to comply with the 'Blue Book'

Traffic Coordinator Report No. 8/10

1. PROPOSED ALTERATIONS AND ADDITIONS TO EXISTING SHOPPING CENTRE

FILE: DA. 249 / 2010

Proposal: Alterations and additions to the existing Gowrie Street Mall, Gowrie Street, Singleton.

Applicant: Charter Hall Retail

Attachments: **"APPENDIX 1"**: A copy of traffic relevant extracts from the traffic impact assessment by Traffix traffic and transport planners. The selected extracts are as per the following table:

SEE Extracts		
Part	Sub Part	Page
6 traffic impacts	6.1 – 6.6	13 - 18
8. conclusions		21 - 22

"APPENDIX 2": A plan of the development titled "PROPOSED GROUND FLOOR"

Charter Hall Retail is proposing to add floor area to the existing Gowrie Street Mall. The existing Mall has a total floor area of 11,018 m² with 619 off-street car parking spaces. The proposal is to increase this to 24,628 m² with 871 off-street car parking spaces.

The traffic report submitted appears to be a token. It contains errors as well as vague statements. No cases are presented for a number of things proposed. The assessment purely focuses on the needs or desires of the development proposal to the exclusion of any adjoining activities. It is almost as if the authors worked with the philosophy that the town must fit in with the development not the development must fit the existing town.

Errors in data, and vague statements, detected in the traffic assessment, are detailed in the following table:

Facts in Question		
Part	Error or Vague Statement	Correction or Question
3.2	States that Ryan Avenues speed limit is 50 km/h	Its 60 km/h
6.6	Gives the impression that there is an existing taxi rank on Gowrie Street	The existing taxi rank is within the Gowrie Street Mall just outside the northern access doors

The development site has frontages to the following roads or access ways:

1. Gowrie Street
2. John Street
3. Burns Lane
4. Ryan Avenue
5. ROW

Traffic Coordinator Report No. 8/10

The proposed alterations to what currently exists on these road or ways are detailed in the following series of tables:

Table 1. Proposed Action or New Facilities Gowrie St		
Street	Action or Facility	
Gowrie Street	Removal of bus zone	6.6
	Removal of right turn ban at John St	6.4
	Closure of existing exit point from Gowrie Street Mall	6.4
	Provision of a marked pedestrian crossing.	6.4
	Install a Stop control at John Street.	6.4

Proposed Bus Zone Removal

This begs the question why shift the Gowrie Street bus zone. Its current location is a practical location for servicing two shopping complexes, the Gowrie Street Mall and the Singleton Plaza. A bus zone located around the corner in John Street will amount to a disservice for Singleton Plaza customers.

Table 2. Proposed Action or New Facilities John St		
Street	Action or Facility	
John Street	Provide a bus stop	6.6
	Provide a taxi rank	6.6
	Provide a marked pedestrian crossing.	6.4

Provide a Bus Stop

The site selected for the bus stop would impede north bound through traffic on John Street and reduce the efficiency of the existing right turn arrangement into York Street. If the developer desires a bus stop at this site they would have to widen the carriageway to preserve the separate right turn lane for York Street as well as the through lane on John Street.

Provide a Taxi Zone

The proposal for a taxi zone onto the western side of John Street will have the same negative effects as providing a bus stop in the same area.

The current taxi rank in the Gowrie Street Mall is internal at a point close to the northern doorway. A rank on John Street would be remote from the Mall forcing patrons of the Mall to walk longer distances while loaded with shopping. Also boarding taxis on street would result in problems with discarded shopping trolleys on the street.

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Provide a Marked Pedestrian Crossing

The crossing proposed mid block in the York Street, Gowrie Street block will eliminate five to six existing on-street parking spaces in front of the existing shops on the eastern side of John Street.

This crossing would better serve traffic flow in the subject area if this crossing was placed on the northern side of York Street

Table 3. Proposed Action or New Facilities Burns Ln

Street	Action or Facility	
Burns Lane	Make narrow section one way westward	6.3, 8.00
	Narrow carriageway and provide a wider footway.	6.3

Burns Lane

It is proposed to make Burns Lane one way westward. This will disadvantage, the collection of waste, from the businesses in the complex that have a frontage to Burns Lane. Waste bins are left for collection on the northern side of the footway of Burns Lane. A one way in a westerly direction will mean that these bins would have to be deposited on the opposite side of Burns Lane. Presenting, a, disservice for the parties who put the bins out. On Monday night 9 August 2010 14 bins were placed out along the northern footway of Burns Lane.

Table 4. Proposed Action or New Facilities Ryan Ave

Street	Action or Facility	
Ryan Avenue	Close existing access point to customers	8.0
	Provide a seagull at the main access road onto Ryan Avenue. (Burns Ln) Redesign of delivery vehicle exit point.	6.4

Ryan Avenue

The redesign of the truck exit is not compatible for semi trailers who wish to travel south on Ryan Avenue. Their exit turn movement will put them across the centre line of Ryan Avenue putting them into conflict with vehicle travelling north on the western side of Ryan Avenue. A most unsafe, and undesirable vehicle conflict.

Large vehicle approaching subject exit point should be straight and at a right angle to the Ryan Avenue carriageway to facilitate vehicle operators sighting along Ryan Avenue in both directions.

Traffic Coordinator Report No. 8/10

Table 5. Proposed Action or New Facilities ROW		
Street	Action or Facility	
ROW	Use this ROW. As, the principal access from the north.	Plan
	Provide a marked pedestrian crossing.	Plan

R.O.W.

This existing R.O.W. (right of way), illustrated as Lot 12 in D.P.1005149, is proposed to be the main access two way aisle from the north. It is possible that traffic from both John Street via Burns Lane and Ryan Avenue via Burns Lane to use this corridor. It will be a possible source of congestion as large vehicles service shops that back onto this. For example the large rigid trucks that:

- Services the butchers shop along this strip
- Empty the waste bins from shops along this strip. On Monday night 9 August 2010 eight bins were placed out along the ROW.

To depart from this ROW in peak times these large rigid vehicles will either have to:

- Reverse out to Burns Lane
- Go in a forward direction through the shopping centres main traffic aisle to Gowrie Street.
- Weave through the shopping centres northern section of car park to get back onto Burns Lane.

The above options will result in more exposure to conflict with pedestrians put these large vehicles.

The traffic assessment stated that the proposed development would generate a peak hour traffic volume of 966 vehicle movements. Estimates of where arrivals and departures for various abutting roads were expressed in percentages on page "14" of the assessment. These percentages calculated as numerical volumes are presented in the following table:

Table of Estimated Traffic Data, Peak Time Arrival and Departure			
Location			
Street	Ingress	Egress	Source
Gowrie St	386	483	Page 14 traffic impact assessment
Ryan Ave	579	386	Page 14 traffic impact assessment
Burns Ln	0	96	Page 14 traffic impact assessment

Solid Waste

No mention is made to how solid waste will be stored, picked up and transported from the proposed redevelopment. So, waste service vehicle access and movements could not be assessed. The applicant should be asked to supply this data if it is available before determination.

Traffic Coordinator Report No. 8/10

RECOMMENDED that, in the planning assessment of the proposed redevelopment, of the existing Gowrie Street Mall, bounded by Gowrie Street, John Street, Ryan Avenue and Burns Lane. The applicant be requested to redesign, the proposal to remove traffic proposals identified with the potential to have negative impacts on other parties in the locality of the proposed development. The following should be reconsidered:

- i) The location of the bus stop and taxi zone on John Street near York Street.
- ii) The proposed mid block marked pedestrian crossing on John Street in the York Street to Gowrie Street block.
- iii) One way traffic in the narrow part of Burns Lane.
- iv) Provision in the ROW access for 12 metre large rigid trucks to turn around and travel in the opposite direction..
- v) Adjustment of the egress point for delivery vehicles so that the large vehicle can pass through it at a right angle with the carriageway of Ryan Avenue to prevent crossing the marked centre line of Ryan Ave.
- vi) The provision of an internal taxi rank proximal to a pedestrian access door.
- vii) Assessment of warrants for any on-street marked pedestrians crossings proposed
- vii) Solid waste storage and removal.

M LAUNDERS
TRAFFIC COORDINATOR

19/8/2010



Singleton & Districts
Disability Advisory Committee

PO BOX 314
Singleton NSW 2330



Enquiries: Wendell Peacock
6578 7215

The General Manager
Singleton Council,
Mr. Scott Greensill,

3/08/2010

Dear Sir,

In reference to the DA 249 / 2010, may we make the following submission:

Compliance with all relevant Australian Standards: With particular reference to:
Disability (Access to Premises-Buildings) 15th March 2010.

We believe a review of the Singleton Disability Access Plan with the view of incorporating any of the relevant accesses into this development.

We believe the following points need to be addressed prior to any approval being given:

1. All parking spaces needs to be compliant with current Australian Standards, and an extra 2 disabled car spaces made available on both Level 1 and Level 2.
2. The pedestrian walkways through the ground level car parking needs to be raised , and or speed humps installed on both sides of the walkways , and adequately signposted.
3. The proposed position of the bus shelters need to be relocated to a more suitable location. The enormous volume of traffic in the John / York Street vicinity will create enormous problems for not only people with a disability, but all users.
4. The taxi ranks and emergency vehicle parking needs to be adjacent to the main entrance.
5. The 5 car spaces adjacent to the proposed pedestrian crossing in Gowrie Street need to be removed. These spaces, if allowed to remain will create a major safety hazard in regard to visibility.
6. We believe compliant "pedestrian crossing" lights need to be installed at the intersection of York and John St. Given the high volume of pedestrian traffic in the vicinity at the moment, it is very difficult for the disabled and elderly to cross York St., and once the new complex is completed, it will be even more difficult. If we do



An Advisory Committee to Singleton Shire Council

not put sensible, practical solutions in place now, it will be too late if someone gets seriously hurt.

7. A compliant disabled toilet needs to be installed in or near the "mini major", as we do not believe the proposed WC's are enough to adequately support the complex.
8. As part of the Singleton Disability Access Plan , the southern side of Gowrie Street was identified as needing a compliant footpath and aprons to establish access into the Rose Point Complex and Rose Point Sporting fields . We hope this section of the access plan can be incorporated into the development.
9. A compliant footpath and aprons installed around the northern side of the Woolworths petrol station to allow access from the new complex , across Ryan Avenue and into the northern end of the sporting fields .

Given that the Rose Point Industrial Complex cannot be safely accessed (for pedestrians) at this point, and the fact that our major sporting venue (Rose Point Park) cannot be safely accessed across Ryan Avenue at any point, we believe it is essential that safe pedestrian access across Ryan Avenue, at 2 different points, needs to be done in conjunction with this development.

The fact , that in 2010 , we cannot safely walk from our major shopping precinct across one road to our major sporting venue , is an absolute disgrace .

We now have the opportunity to ensure access for all our pedestrians, but especially for the aged and disabled.

We need to create 2 safe crossing points that will ensure we fulfill both our legal and moral obligations.

The Disability Advisory Committee, like the majority of Singleton residents wants this development to go ahead. We now have the opportunity, as there will be major disruptions during the construction phase , to work with all relevant stake holders , ie: DAC , traffic committee, sporting organizations , etc to ensure the long term commitment to not only disability services , but all pedestrians in Singleton .

We are available at any time to meet with either the developers or representatives of Singleton Council if either party believes we can be of assistance.

Sincerely Yours,

N.MacPherson.
Chairman SDDAC.
65724465
0432 095 940.

